



Deerfield Beach Community Redevelopment Agency

AGENDA

Tuesday, August 31, 2010, 6:30 P.M.

City Commission Chambers, Deerfield Beach City Hall

CALL TO ORDER AND ROLL CALL

APPROVAL OF MINUTES

APPROVAL OF THE AGENDA*

August 31, 2010

GENERAL ITEMS

1. CRA Board Resolution Approving FY 2011 Budget*
2. Discussion regarding Draft FY 2011-2015 Capital Improvement Plan
3. Request to award The Cove Shopping Center Parking Lot Improvements Project Contract to West Construction*

BOARD/ADMINISTRATION COMMENTS

PUBLIC INPUT

ADJOURN

* Indicates an Action Item

(Next Meeting: Tuesday, September 21, 2010, 6:00 PM unless otherwise determined)

REQUESTED ACTION:

Approve FY 2010-2011 CRA Budget via resolution.

SUMMARY EXPLANATION/BACKGROUND:

Florida Statutes Chapter 189.418 stipulates that the CRA Board must adopt the budget via resolution each fiscal year prior to adoption by the Governing Body (City Commission).

Two changes have been made since the proposed budget was presented to the Board at its August 17, 2010 meeting:

- Specific designation of capital improvement funds of \$320,000 for the reconstruction of the alley to the south of the Cove Shopping Center
 - Funding in the amount of \$135,000 to reimburse the city for the cost of capital improvement project management as follows: Dean Payne - \$62,430 (Hillsboro Blvd., Pier Renovation), Bernard Buxton-Tetteh (A1A, Cove Shopping Center, Main Beach Parking Lot)- \$42,880, Charles DaBrusco (Oversight)- \$29,690
-

ATTACHMENTS:

1. CRA FY 2010-2011 Budget
2. Resolution

BUDGET LINE ITEM SUMMARY
OCTOBER 1, 2010 TO SEPTEMBER 30, 2011

COMMUNITY REDEVELOPMENT AGENCY FUND 190
NON-DEPARTMENTAL
NON-DEPARTMENTAL 8000-552

ACCT NO.	ACCOUNT DESCRIPTION	FY 2010 BUDGET	FY 2011 REQUEST	INCREASE (DECREASE)	% CHANGE
10-01	Regular Salary	51,647	394,853	343,206	664.5%
10-03	Longevity	-	1,704	1,704	-
10-06	Automobile Allowance	-	4,000	4,000	-
11-01	FICA	3,951	30,654	26,703	675.9%
12-04	ICMA Pension Plan	3,413	29,245	25,833	757.0%
32-99	Other Contractual Services	297,000	325,000	28,000	9.4%
35-04	Office Supplies	400	400	-	0.0%
35-09	Professional Publications	300	300	-	0.0%
35-13	Minor Tools, Equip, Hdwe	5,900	2,100	(3,800)	-64.4%
35-74	Special Events	-	275,000	275,000	-
39-01	Travel and Training	5,000	5,000	-	0.0%
39-02	Printing	1,500	1,500	-	0.0%
39-21	Advertising	1,500	10,000	8,500	566.7%
39-27	General Admin Charge	32,955	32,955	-	0.0%
39-35	Dues and Memberships	4,000	-	(4,000)	-100.0%
39-59	Commercial Façade Improv Loan Prog	600,000	600,000	-	0.0%
63-01	Cove Parking Lot	3,800,000	3,800,000	-	0.0%
63-02	Hillsboro Streetscape	510,000	510,000	-	0.0%
63-03	Pier	3,500,000	3,500,000	-	0.0%
63-04	Infrastructure & Capital Improvements	503,890	503,890	-	0.0%
63-05	North A1A Streetscape	1,875,000	1,420,000	(455,000)	-24.3%
63-06	Beach Enhancements	-	90,000	90,000	-
63-07	Real Estate Acquisition	-	1,000,000	1,000,000	-
63-08	Cove Shopping Center Alley Improvements	-	320,000	320,000	100.0%
90-01	Transfer to General Fund	688,824	686,794	(2,030)	-0.3%
90-03	Transfer to Insurance Services Tr	4,000	4,000	-	0.0%
Total		11,889,280	13,547,395	1,658,115	13.9%

RESOLUTION NO. 2010/_____

**A RESOLUTION OF THE COMMUNITY REDEVELOPMENT AGENCY
OF THE CITY OF DEERFIELD BEACH, FLORIDA, ADOPTING THE
FISCAL YEAR 2010-2011 BUDGET**

WHEREAS, Florida Statutes Chapter 189.418 stipulates that the CRA Board must adopt the budget via resolution each fiscal year prior to adoption by the Governing Body (City Commission); and

WHEREAS, the CRA Board has reviewed the FY 2010-2011 Budget and find it consistent with the Deerfield Beach CRA Plan goals and objectives;

NOW, THEREFORE, BE IT RESOLVED BY THE COMMUNITY REVELOPMENT AGENCY OF THE CITY OF DEERFIELD BEACH, FLORIDA, AS FOLLOWS:

Section 1. The above referenced "Whereas" clauses are true and correct and made a part hereof.

Section 2. The Deerfield Beach CRA Board of Directors hereby approves the FY 2010-2011 Budget.

PASSED AND ADOPTED THIS _____ DAY OF _____, 2010.

PEGGY NOLAND, CHAIR

ATTEST:

ADA GRAHAM-JOHNSON, MMC, CITY CLERK

REQUESTED ACTION:

Discussion regarding the 5 Year Capital Improvements Plan (CIP)

SUMMARY EXPLANATION/BACKGROUND:

In order to plan the next five years' activities of the CRA, staff asked Chen and Associates to prepare a 5 Year Capital Plan. This plan is a detailed report on the projects that the CRA anticipates implementing over the next five years, provides a narrative of and justification for each of the projects and is accompanied by a project budget. Projects were prioritized and scheduled on a timetable that is manageable for staff to implement.

Beginning next Fiscal Year, the Capital Improvements Plan will be fully integrated into the CRA budget with capital projects appearing as line items within the budget. This type of integration will allow staff to accurately benchmark progress, CRA accomplishments and return on investment.

ATTACHMENTS:

1. Deerfield CRA 5 Year Capital Plan

The City of Deerfield Beach Community Redevelopment Area (CRA) is located within the central area of the City limits and encompasses approximately 468 acres. The CRA area surrounds the City's main arterial of Hillsboro Boulevard between Federal Highway and Ocean Way. The north and south boundaries of the CRA area varies from east to west. The CRA area includes a limited number of parcels to the west of the Intracoastal Waterway, which includes Sullivan Park just north of Hillsboro Boulevard. The bulk of the CRA area is located to the east of the Intracoastal Waterway and expands along State Road A1A. The CRA area extends north to the City Limits at NE 7th Street and south to SE 3rd Street at its furthest extents. The CRA area includes mature neighborhoods and commercial areas with a mixture of older buildings and more recent developments. The older buildings may be subject to redevelopment in the coming years. The limits of the CRA are displayed within the Figure 1.3 above.

1.4 Existing Zoning & Land Uses

The existing land use within the CRA area is a mixture of commercial, residential, institutional, and recreational. The majority of the area is composed of either public right of way or retail/commercial area. Various other land uses, such as multifamily residential, residential office, and community facilities, can also be found within the CRA area. The existing zoning of the parcels within the CRA area defines various building and development requirements of each property, including maximum density and building heights. Zoning of each individual parcel within the CRA is displayed within Figure 1.4 below.



Figure 1.4 – CRA Zoning Categories

The majority of the parcels in the CRA area currently have viable structures and therefore the potential for infill projects that will impact the infrastructure in the area is low.

1.5 Future Zoning & Land Uses

The future land use within the CRA area is much more homogeneous than the existing land use categories. Commercial activities are located along the Hillsboro Boulevard corridor west of the Intracoastal Waterway, and centered along the “S” Turn of State Road A1A/NE 2nd Street east of the Intracoastal. There is an emphasis on high density residential on the barrier island with recreational open space and limited community facilities to service said residential.

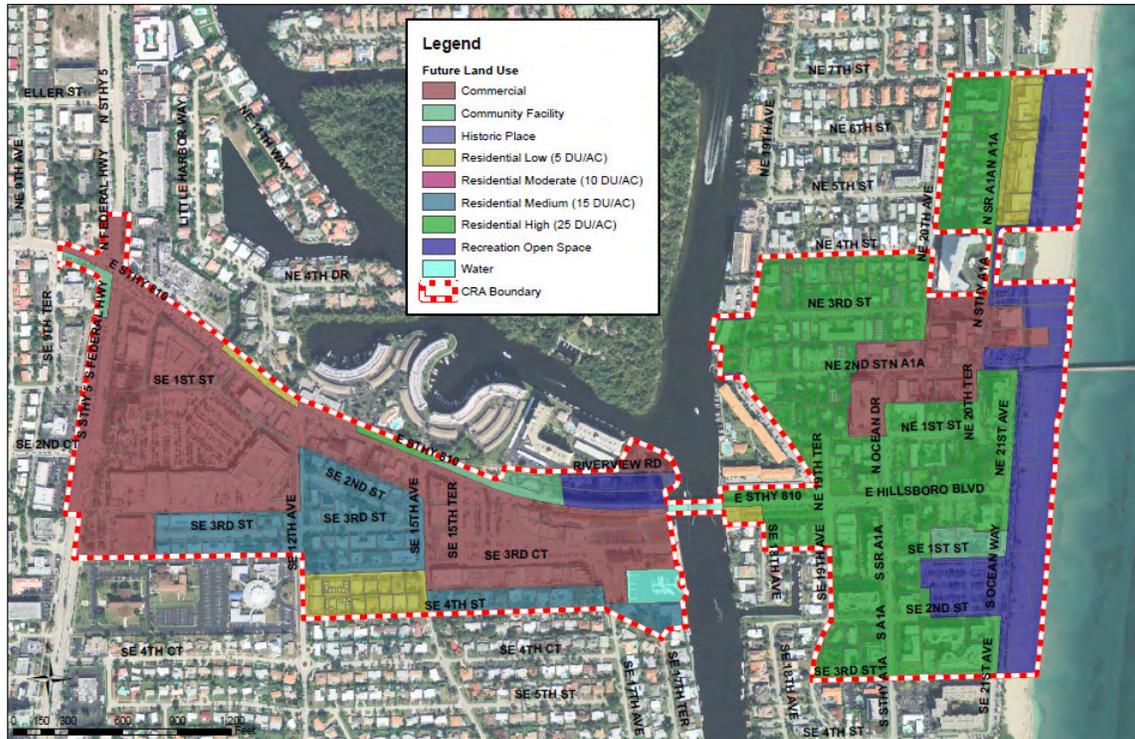


Figure 1.5 – Future Land Use

1.6 Population Changes

When the CRA area undergoes revitalization through redevelopment, population increase within the area should exceed regional population growth rates. According to the US Census Bureau, the population growth within urban areas will average 1.1% annually over the next 20 years across the nation. Based on further data from the US Census Bureau, the population within Broward County is currently growing at 1.2% each year. According to the 2007 Community Redevelopment Plan Update for the Beach/Cove Area, this area’s population was expected to grow from 6,078 to 6,175 from 2005 to 2010. This estimated population growth appears to be in line with other growth projections for the State of Florida. The average population growth through 2030, which were estimated by different sources, is listed within the Table 1.6 below.

Table 1.6 – Future Population Growth Rate within Florida

Source	Average Annual Growth (Statewide through 2030)
South Florida Regional Planning Council	1.69 %
Florida Demographic Estimating Conference	1.57 %
Economic & Demographic Research	1.52 %

The future population growth is important for estimating the future demand on existing infrastructure elements, specifically the water distribution system and the sanitary sewer system. Although the general population projections along the land use changes can be used to approximate the future demands on various infrastructure elements, the CRA may consider a more comprehensive analysis of the future population growth within the CRA. Since the future land use changes within the CRA could allow an increase in maximum density within portions of the CRA, the future population growth within the CRA could be greater than the general population projections. The impacts of these future density increases on the existing infrastructure, such as future water and sewer demands, was not been studied as part of this report.

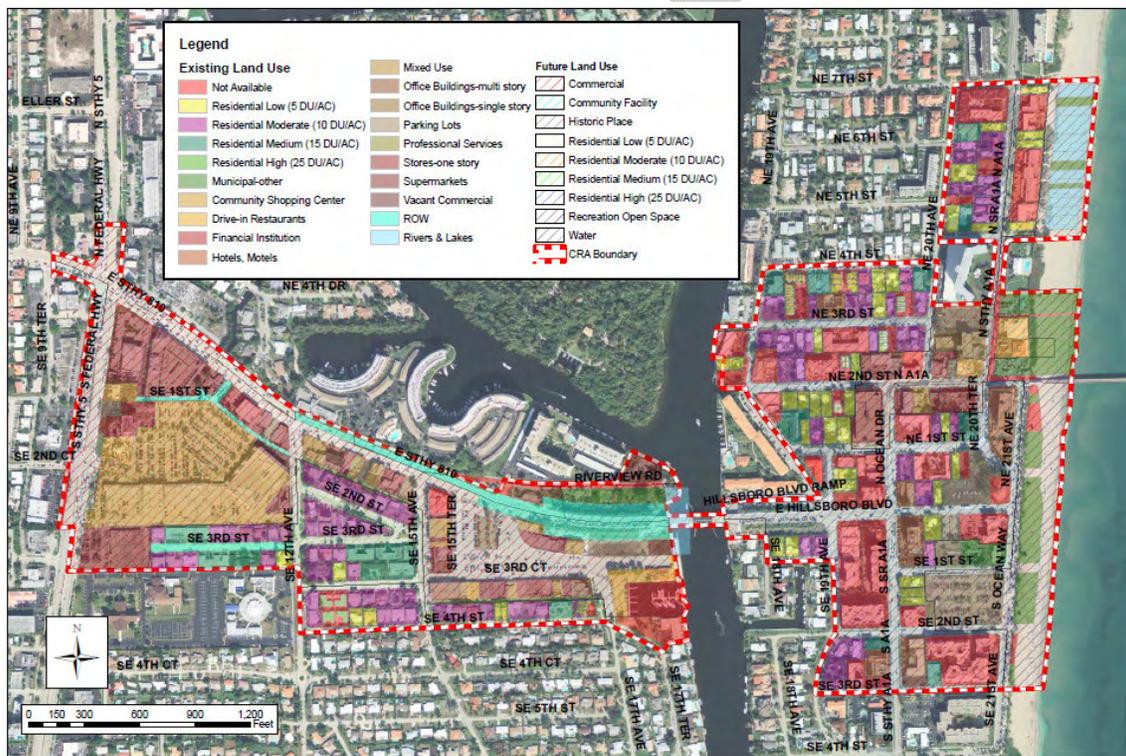


Figure 1.6 – Land Use Comparison

Based on all of these factors, the proposed infrastructure improvements are considered for their net impacts to the current environment for the residents living within the CRA, the resident living within the City in close proximity to the CRA, the visitors to the City that frequent the local amenities, and the local businesses. For the purposes of this report, these future infrastructure improvements are grouped into the following categories:

- Water and Sewer Improvements
- Drainage Improvements
- Sidewalk and Streetscaping Improvements
- Parking Improvements
- Improvements to Existing Park Facilities
- Lighting Improvements

The following sections of the report discuss the potential infrastructure improvements within the CRA to be implemented in the next five year period along with the project budgets in 2010 construction dollars based on all the available information.

DRAFT

2 WATER IMPROVEMENTS

The primary purpose behind the inclusion of water system improvements with a CRA Capital Improvement Plan is the relationship between urban density and the demands on the existing utility infrastructure. The capacity of the water and sewer systems will need to be increased as the population within the service area in order to match the demand. In many cases, water and sewer infrastructure does not merely need to be upsized to accommodate redevelopment, but rather entirely new methods of managing demand are required.

2.1 Existing Water Distribution System

The City of Deerfield Beach provides potable water to all parcels within the CRA area via its water distribution system. The bulk of the water is supplied from the City's Water Treatment Plant, which is located at 290 Goolsby Boulevard. The City of Deerfield Beach utility atlas was reviewed to assess the extent of the water distribution system serving the CRA area. The potable water supply is provided to the CRA area by an existing pipe network of water distribution and transmission mains. The transmission mains act as the backbone of the water distribution system. The transmission mains are larger diameter pipe with the capacity to provide necessary flow to the CRA area from the supply source and maintain a minimum pressure within the pipe network. The majority of the transmissions mains, which serve the CRA area, are either 10-inch or 16-inch diameter and provide good coverage by surrounding the perimeter of the CRA area. The transmission mains are interconnected with the distribution mains, which provide water service to each parcel and fire flow to existing fire hydrants. The distribution mains, which serve the CRA area, are 8-inch diameter and less. Based on review of the water utility atlases, there are approximately 8,400 linear of water main within or immediately adjacent to the CRA area. The breakdown of pipe diameters within the CRA area is listed within Table 2.1 below:

Table 2.1 – CRA Water Distribution System

Pipe Diameter	Pipe Length	Percentage
20-inch	75 LF	0.9%
16-inch	805 LF	9.6 %
12-inch	440 LF	5.2 %
10-inch	1,235 LF	14.7 %
8-inch	1,095 LF	13.1 %
6-inch	3,260 LF	38.9 %
4-inch	95 LF	1.1 %
2-inch	1,375 LF	16.4 %

Based on a review of the City's Water Master Plan, there are no planned capacity enhancements in the CRA area. Many municipalities have been replacing existing 2-inch water main due to their limited transmission capacity and inability to maintain consistent pressures to the users. Upon the completion of a pressure analysis of the existing system,

the City may want to consider the eventual replacement of these 2-inch water mains, which make up 16% of the water distribution system within the CRA.

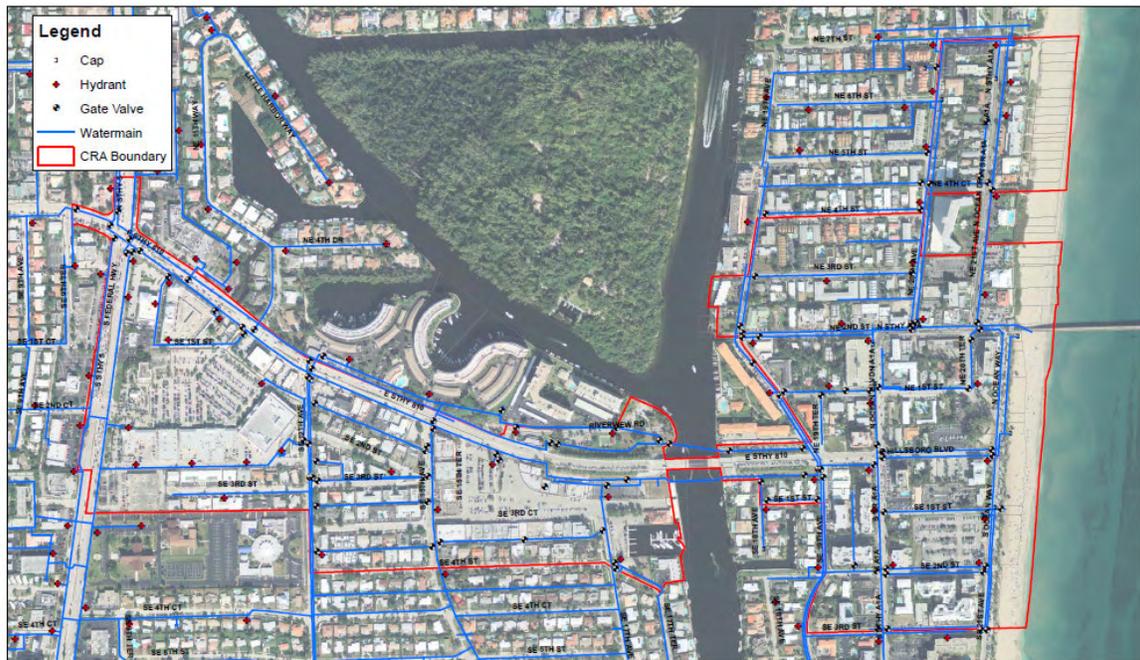


Figure 2.1 – Existing Water Distribution System

2.2 Fire Hydrant Coverage Upgrade Project

Based on our review of the utility atlas, the spacing of existing fire hydrants currently provides an adequate coverage according to recommendations by Broward County. The fire hydrants should be spaced with approximately a 300 feet radius. When each existing fire hydrant was assigned a coverage radius of 300 feet, there are only a few small gaps in the fire safety provided to CRA area by the existing water system. Based on our review of the existing system, approximately 9 new fire hydrants can be installed to adequately improve fire protection within the CRA. The locations of the existing fire hydrants are displayed in light blue along with the proposed fire hydrants in dark blue within Figure 2.2 below.

Based on our review of the drainage atlas, the existing stormwater system within the CRA consists of 136 catch basins, 16 drainage manholes, and 8 outfalls to surface waters. The existing stormwater management system within the CRA is displayed within Figure 3.1.2 below. The purpose of the stormwater system is to provide flood protection to public right of way areas while ensuring water quality treatment of stormwater runoff before it reaches surface waters via the outfalls. The stormwater management system must meet the level of service criteria defined by the regulatory agencies.



Figure 3.1.2 - Existing Drainage Map

Cove Gardens and the Cove Shopping Center “alley” are the only areas with overall substantiated improvement.

3.2 Regulatory Requirements

The regulations of Broward County Environmental Protection and Growth Management Division (BCEPGMD), South Florida Water Management District (SFWMD), and Florida Department of Transportation (FDOT) were reviewed to establish any limitations the regulatory agencies may place on the expansion of the City’s stormwater system. Based on our review of this information, the drainage requirements for the study areas are regulated by the South Florida Water Management District (SFWMD) and by Broward County Environmental Protection and Growth Management Division (BCEPGMD). These requirements are outlined within the following sections.

3.2.1 South Florida Water Management District

Quality Criteria

The water quality criteria were established to reduce the pollution impact due to stormwater runoff on surface water bodies. The initial volume of runoff during a rainfall event typically contains higher concentrations of pollutants and sediments which have

been flushed from the existing ground surface within the drainage basin. SFWMD requires that the overall system retain/treat a volume equal to 1.0 inch of stormwater runoff times total basin area or 2.5 inches of stormwater runoff times the impervious area located within the basin, whichever is greater. The volume of stormwater which must be treated was determined for each drainage basin within the CRA. This volume of stormwater runoff must be prevented from entering adjacent surface waters via positive outfalls and be diverted for infiltration into the groundwater via surface swales, retention areas, or exfiltration trenches.

Quantity Criteria

The water quantity criteria were established to define the volume of stormwater runoff which must be disposed to prevent flooding within the drainage basin. According to SFWMD requirements, a drainage system within the public right of way must be designed to provide flood protection to public roadways. The drainage system must have adequate disposal capacity to ensure the existing public roadways are not flooded during a specified rainfall event. Based on SFWMD criteria, the maximum flood stage during a SFWMD 5-year, 24-hour rainfall event for the area must not exceed the centerline elevations of public roadways within each basin. Improvements to the existing drainage system are not permitted to increase the off-site discharge rates into surface waters through existing outfalls. The discharge rates from a drainage basin are not allowed to exceed the following quantities:

- Historic discharge rates
- Discharge rates established by previous permits
- Discharge rates specific to SFWMD criteria

3.2.2 Broward County Growth Management and Environmental Protection Department

Governed by Chapter 27 of the Broward County Code of Ordinances, the Broward County Growth Management and Environmental Protection Department has been delegated authority from SFWMD as the primary reviewer of projects outside of unincorporated Broward County and 298 districts. As the delegated authority, the BCEPGMD enforces the same criteria as South Florida Water Management District.

3.3 Cove Gardens Drainage Improvements

Although a comprehensive drainage study was not conducted for the CRA area, the Cove Gardens Neighborhood was identified to be in need of drainage improvements based on our observations and discussions with City staff. The Cove Gardens area is generally bounded by SE 2nd Street on the north, SE 4th Street on the south, SE 12th Avenue on the west, and SE 15th Avenue on the east. The overall drainage area for Cove Gardens consists of 12.5 acres. The inventory of the existing drainage system for this area shows 14 catch basins and 2 manholes which are connected 1,855 LF of stormwater pipe. In even small events, significant ponding is experienced and it persists for several days in some areas because the swale areas are highly impervious as a result of the high percentage of multi-family residential units.



Figure 3.3.1 – Cove Gardens Drainage

The preliminary solution for the drainage issues in the area consist of the addition approximately 8 catch basins and 2,000 linear feet of exfiltration trench systems, which will interconnect the existing drainage and provide additional storage and disposal capacity. The recommended improvements to the stormwater management system within the Cove Gardens neighborhood are displayed within Figure 3.3 above. The drainage improvements should also include a revised roadway cross section, which include the addition of drop curbing for the conveyance of stormwater runoff to the catch basin inlets. The preliminary cost estimate also includes all pavement restoration along with the median improvement at the three-way intersection of SE 2nd Street, SE 3rd Street and SE 15th Avenue as defined below in Figure 3.3.2.

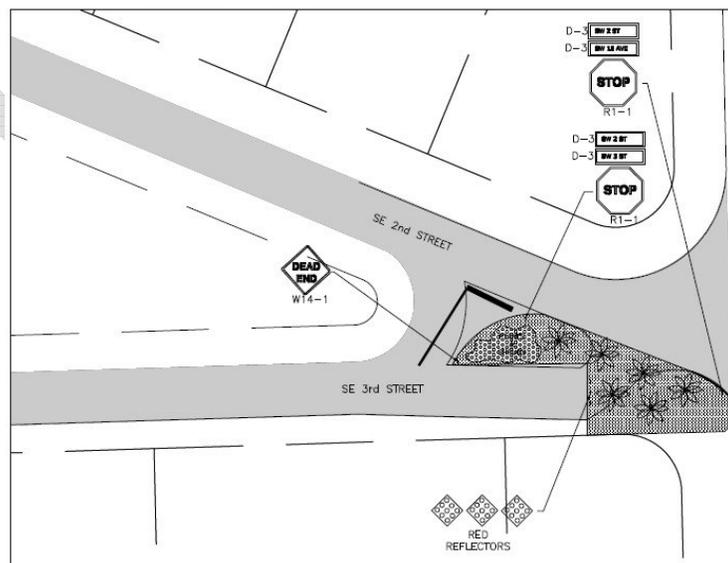


Figure 3.3.2 – Median Improvements

3.4 Cove Shopping Center Alley Improvements

The boundary between commercial and residential properties is always a difficult one to manage. The area on the boundary of the Cove Shopping Center is no exception to this. The area between the commercial and residential properties is separated by a 15' gap of right-of-way and the resulting area has been subject to continued neglect. Improvements to the alley could include the installation of a decorative wall between the residential properties and the commercial properties, drainage improvements, and roadway improvements. Also included in the planning for the Cove Shopping Center Alley Improvements Project would be the completion of the reconstruction of the parking lot at the front of the plaza. The Cove Shopping Center Parking lot is currently contracted for design with Keith and Associates.



Figure 3.4.1 – Cove Shopping Center Alley Improvements

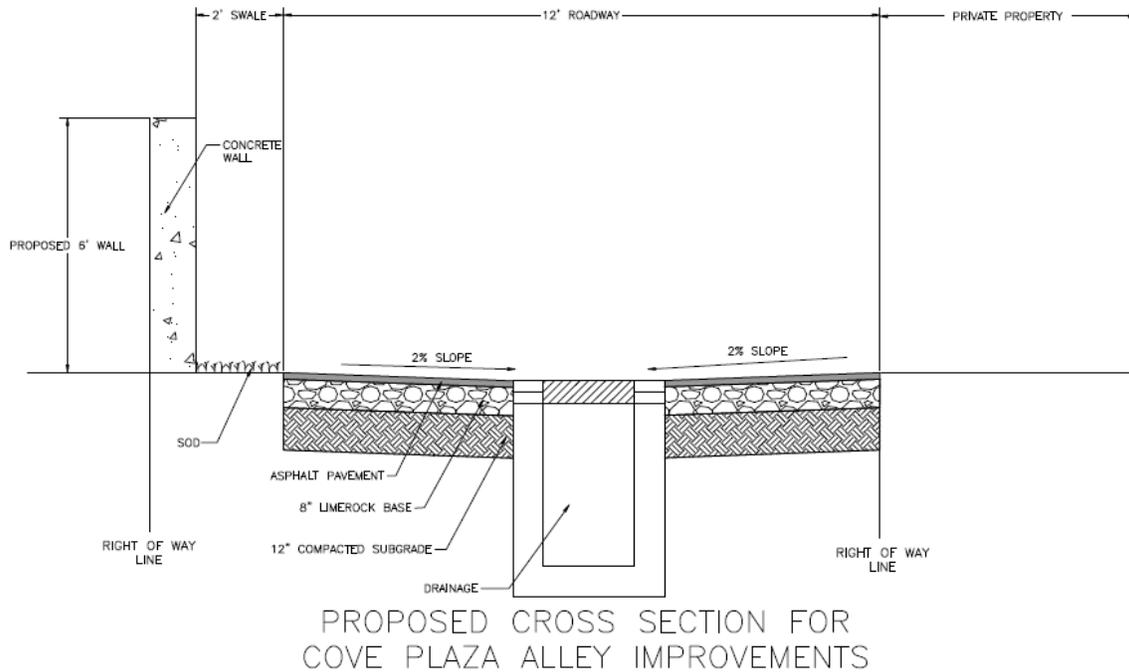


Figure 3.4.2 – Cove Shopping Center Alley Improvements

3.5 Budget Level Cost Estimates

The estimated construction costs for the recommended improvements to the existing drainage system within the CRA are listed within Table 3.5.1 below. The recommended improvements are based on our observations of CRA area and discussions with City staff on areas which are prone to flooding in the past. Please note a comprehensive stormwater model was not developed for the CRA area to verify whether level of service criteria for right of way areas are met by the existing stormwater management system. The recommendations are based on the assumption that any future development, parking lots, and parks will include their own on-site drainage systems as discussed in future sections. These recommendations should be only used for budgetary and planning purposes. Detailed engineering review will be required as each individual parcel undergoes the redevelopment process.

Table 3.5.1 – Estimated Budget for Cove Garden Improvements			
Item	Quantity	Unit Cost	Total Cost
Additional Drainage Structure	8	\$2,500	\$20,000
Additional Drainage Exfiltration / Pipe	2000	\$50	\$100,000
Additional Curbing	2000	\$30	\$60,000
Repaving	3400	\$30	\$102,000
Median Improvements	1	\$35,000	\$35,000
		Subtotal:	\$317,000
		Design Cost:	\$47,550
		Construction Administration:	\$15,850
		Contingency:	\$31,700
		Total:	\$412,100

Table 3.5.2 – Estimated Budget for Cove Plaza Alley Improvements	
Item	Total Cost
Cove Plaza Alley Improvements	\$245,400
	Subtotal: \$245,400
	Design Cost: \$36,810
	Construction Administration: \$12,270
	Contingency: \$24,540
	Total: \$319,020

4 SIDEWALK AND STREETSCAPING IMPROVEMENTS

Safe and passable sidewalks are a key infrastructure element which ensures pedestrian travel as a legit transportation alternative within the CRA. Throughout the CRA area, there are many areas with existing pedestrian sidewalks which connect projects developed on private property. These existing sidewalks have a number of issues, such as improper connectivity and inadequate ADA compliance. There are a number of sidewalk improvements that need to be made within the CRA. South Ocean Way and Hillsboro Boulevard Improvemtns are prime examples of streetscaping's ability to enhance the pedestrian realm and overall aesthetics of an area. The rights-of-way within the CRA vary greatly with respect to both the type of land use that currently resides adjacent to them and the expected vehicular capacity of the roadways contained therein. The opportunities for streetscaping are limited, but some still exist.

4.1 Existing Sidewalks

There are approximately 13,500 linear feet of sidewalks within the CRA in varying conditions. The bulk of the sidewalks were installed well before the passage of the Americans with Disabilities Act and are not compliant with the many requirements of the ADA guidelines, such as failing transverse and cross slopes, inadequate clearance for passage, inadequate warning preceding intersection with vehicular traffic, and other criteria. Although some corrections may not be feasible at certain locations due to the existing conditions, best practices will need to be observed to ensure compliance with ADA requirements.

4.2 Five-Year Sidewalk CIP for ADA Compliance

The City has contracted with Keith and Associates to perform an evaluation of all of the sidewalks within the CRA Area for compliance with the Americans with Disabilities Act. Although the sidewalk study is still underway, preliminary information from the report has yielded a total expected budget of almost \$200,000 for the 8 areas studied to date. One of the 8 study areas is highlighted below in Figure 4.2.

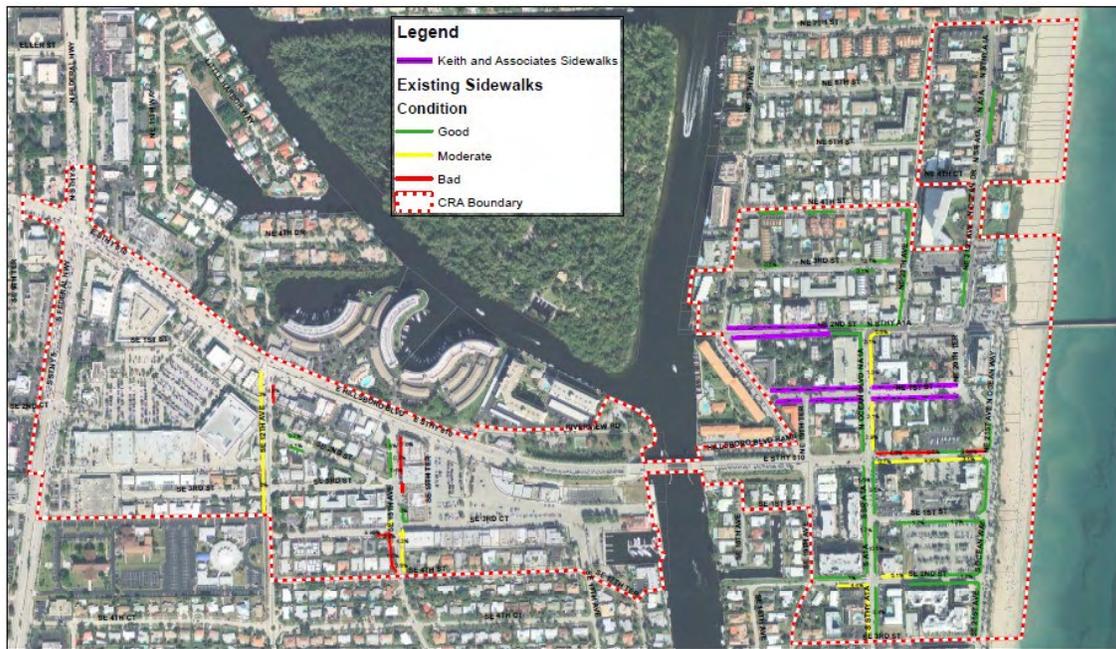


Figure 4.2 – Sidewalk Locations (One Study Area)

4.3 Beach Area Sidewalk Upgrades

The City has requested that a differential cost of upgrading the sidewalks in the beach area to brick pavers to match the South Ocean Way pattern be explored. In a review of the area, this implementation appears to be practical in the following areas:

- Both sides of East Hillsboro Boulevard
- Both sides of SE 1st Street
- North side of SE 2nd Street
- Southwest portion of SE 2nd Street
- North side of SE 3rd Street

These improvements add to the overall aesthetics of the area and provide for a “sense of place” for the residents and visitors in the area. This identifies a Beach District that is a leader for future redevelopment. The recommended sidewalk improvements within the beach area are displayed within Figure 4.3 below.

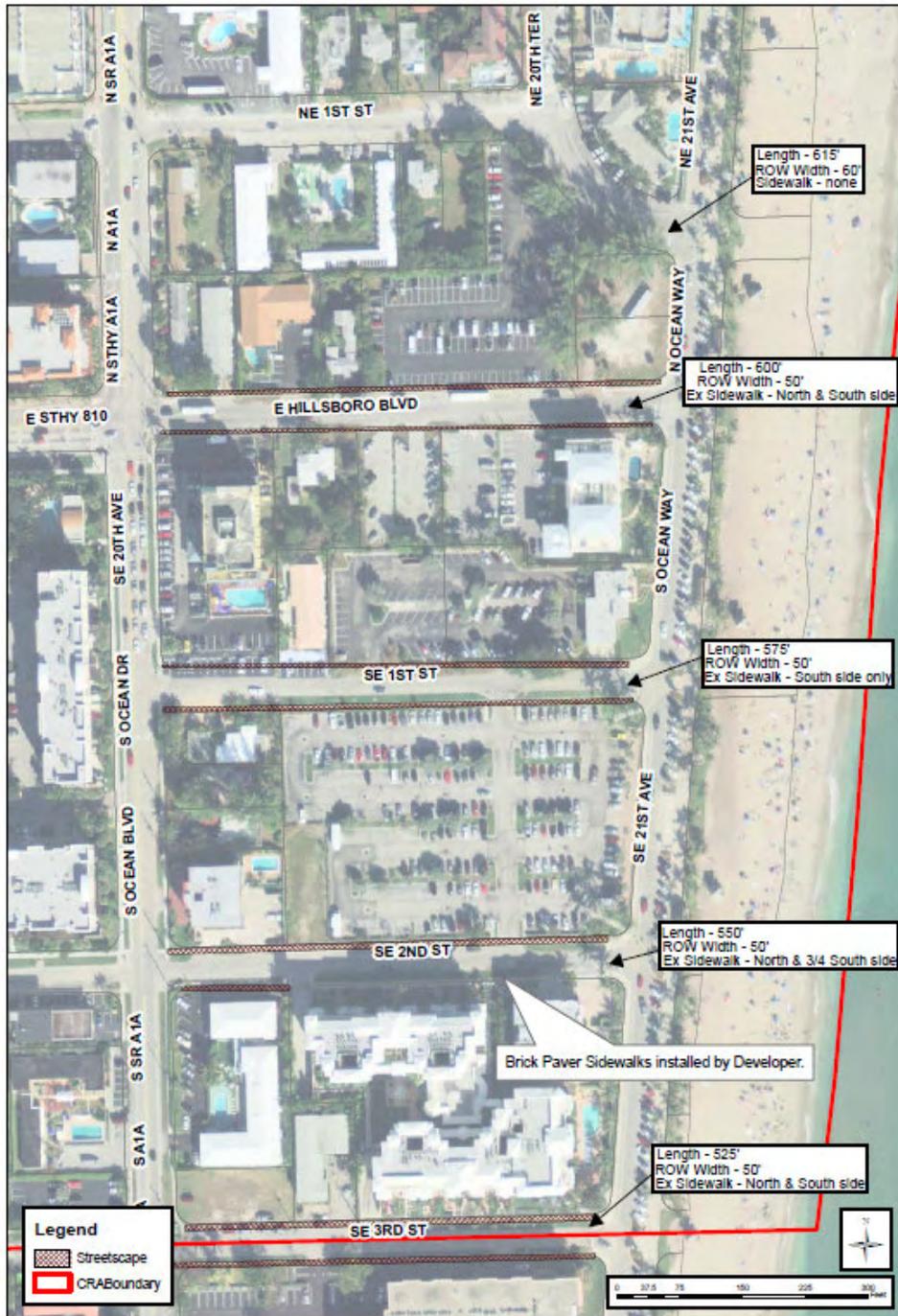


Figure 4.3 – Beach Area Sidewalk Locations

4.4 Hillsboro Boulevard

The City has contracted with Keith and Associates to perform the design for improvements to Hillsboro Boulevard. These improvements are underway and include the installation of enhanced landscaping, decorative lighting, pedestrian friendly paver sidewalks, bus shelters, street furniture, and burying of all overhead utilities.

4.5 Budget Level Cost Estimates

The estimated construction costs for the recommended improvements to the existing sidewalks within the CRA are listed within Table 4.4 below. These recommendations should be only used for budgetary and planning purposes. Detailed engineering review will be required as each individual parcel undergoes the redevelopment process.

Table 4.5 – Estimated Budget for Sidewalk Improvements			
Item	Quantity	Unit Cost	Total Cost
Five Year Sidewalk CIP	5	\$200,000	\$1,000,000
Beach Streetscape - Paver Sidewalk Improvements	2400	\$135	\$324,000
		Subtotal:	\$1,324,000
		Design Cost:	\$198,600
		Construction Administration:	\$66,200
		Contingency:	\$132,400
		Total:	\$1,721,200

5 PARKING IMPROVEMENTS

Due to the public beach, pier, and other amenities, the beach area generates interest that extends far beyond the CRA area and requires adequate public parking options. The need for adequate and safe parking facilities is imperative to the populous of the beach area.

5.1 Existing Parking Facilities

In the beach area, there are approximately 444 public parking spots available in four locations. The public parking is available along South Ocean Way and within three municipal lots along SE 1st Street and East Hillsboro Boulevard as displayed within Figure 5.1 below. In peak times, the amount of parking is insufficient which places a strain on the adjacent properties to manage overflow parking. Additionally, potential future corridor enhancements to State Road A1A may impact available parking for some of the existing hotel and apartment properties and create a need for additional spaces for the current users.



Figure 5.1 – Existing Public Parking

5.2 Parking Lot “A” Improvements

The existing municipal lots located behind the fire station are aged and inadequately connected, which forces traffic to utilize South Ocean Way and SR A1A as a one way pair and adds to the traffic congestion in the area. The combination of the lots will provide for greater circulation, particularly for the traveling public that utilizes the City parking lots to arrive in the Beach District. The recommended improvements to Parking Lot A are displayed within Figure 5.2 below.

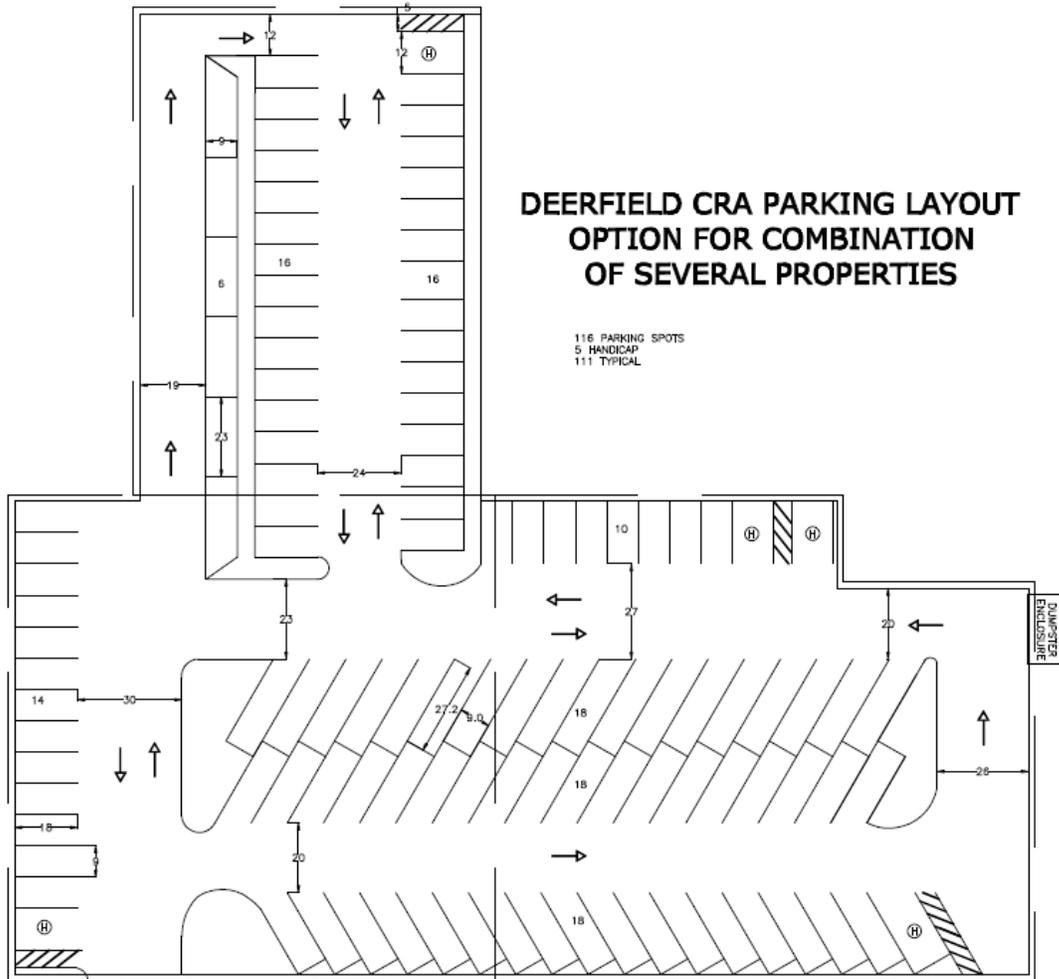


Figure 5.2 – Parking Lot “A” Improvements

5.3 Local Property Purchase

In the current economic cycle, it would behoove the CRA to consider the acquisition of properties in the vicinity of the Beach District for potential parking expansions. While these properties would be eliminated from the tax rolls, the property use would increase to the City’s parking revenues while enhancing the ability for additional persons to frequent the commercial areas in the Beach District. For maximum impact, the properties to be considered should be contiguous with existing parking facilities.

5.4 Northern Parking Lot Expansion

The expansion of the Northern Parking lot after consolidation has the potential of adding 37 spaces as shown in Figure 5.4.

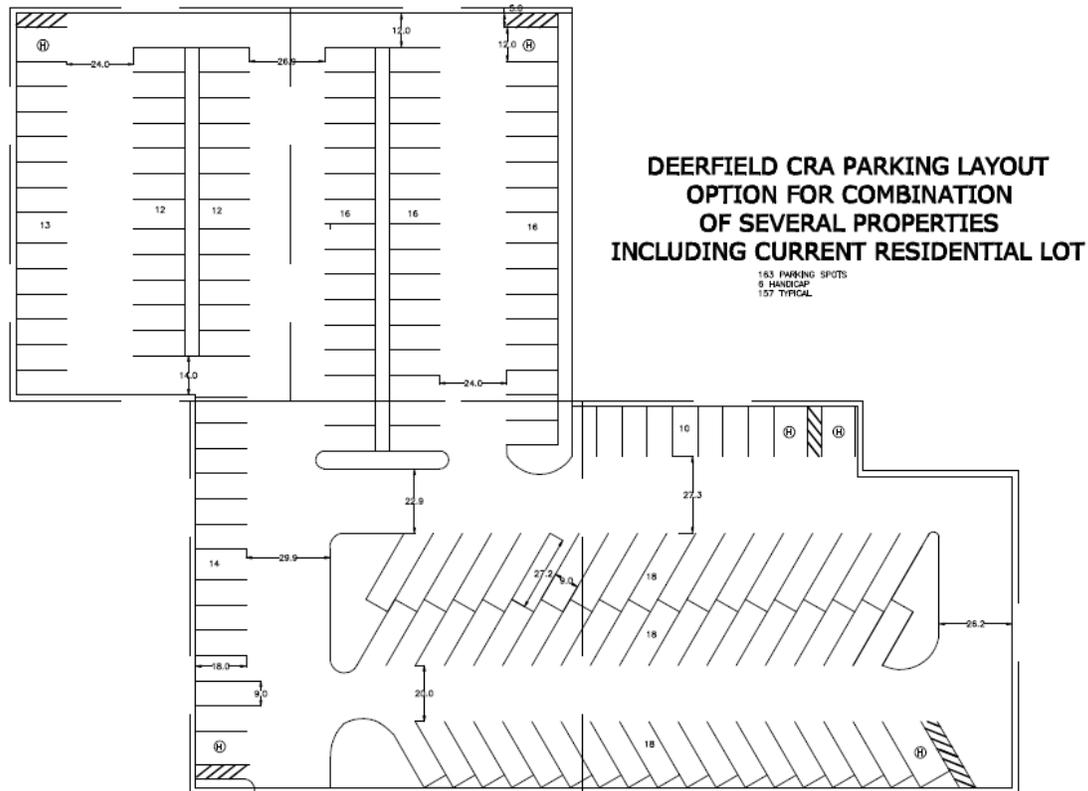


Figure 5.4 – Northern Parking Lot Expansion

5.5 Closure of SE 1st Street and Parking Lot Combination

SE 1st Street east of State Road A1A has limited non-parking related traffic. The potential for a street closure along the potential expansion of parking spaces was considered for this location. There are various layouts for vehicular circulation which have the potential to increase the capacity by up to nearly 100 spaces with the abandonment of the street and the reconfiguration of the existing parking. This project would include providing access from the fire station to South Ocean Way.

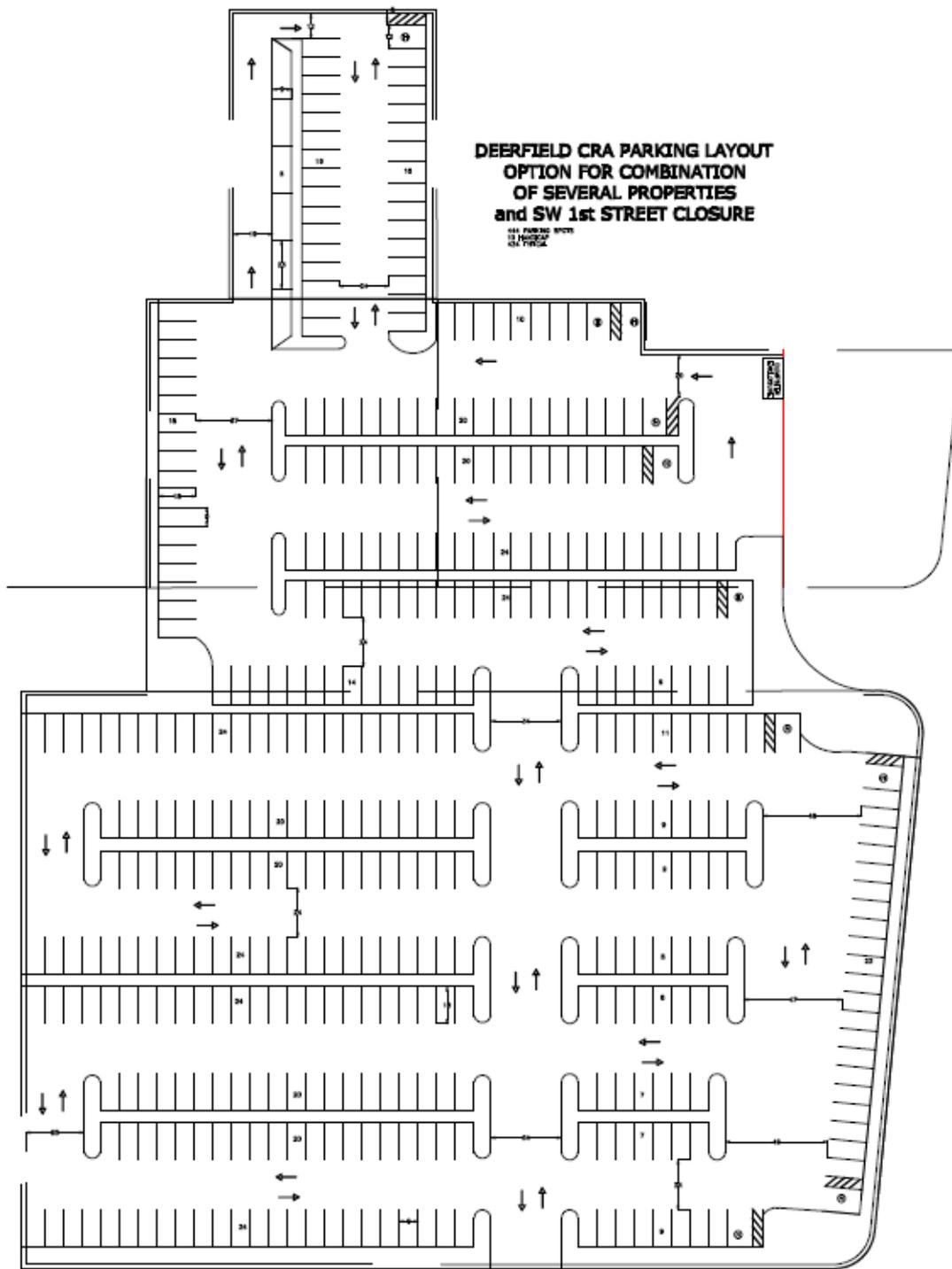


Figure 5.5 – Closure of SE 1st Street Parking Lot Combination

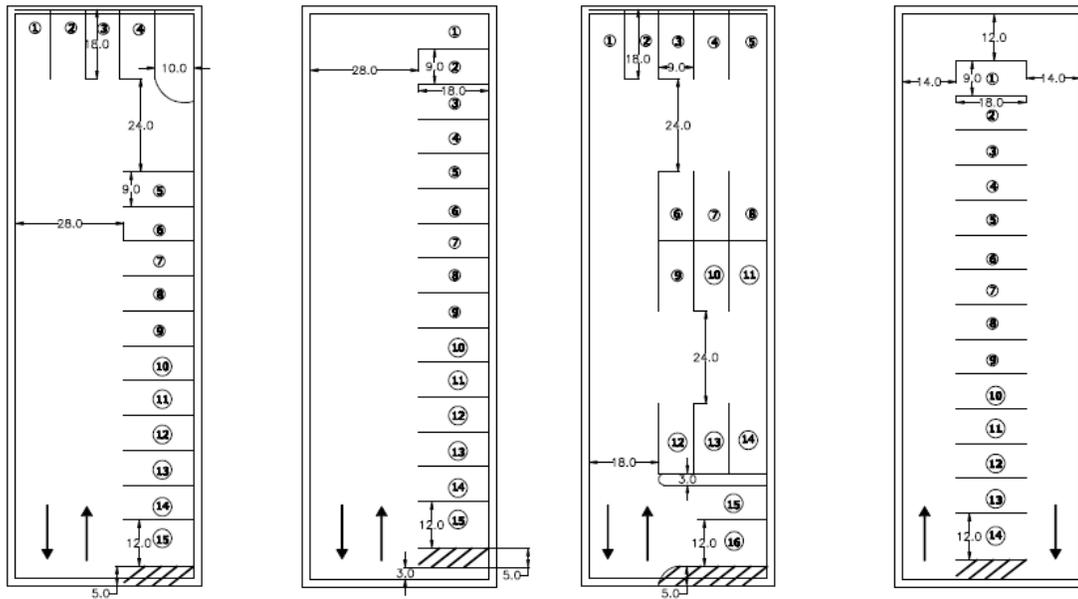
5.6 Residential Lot Purchases

There are several buildings located on lots subdivided for single family homes. According to the property appraiser’s office, these subdivided properties are under a single title. The acquisition of these properties would be a transaction with one owner. An advantage for the CRA to acquire these properties is the ability for short term use of these

lots for overflow parking but to also own them for potential consolidation of properties for potential redevelopment in the future.

5.7 Residential Lot Expansion

The average lot depth in the area is 140 linear feet and the average lot width is 50 linear feet. Based on these lot dimensions, the parking options for a single lot are limited. Several configurations could be utilized, with each lot providing approximately 15 spaces, which are displayed below in Figure 5.7. Each lot would require sit specific design to accommodate the elevation changes in the area as well as the needs of the adjacent properties.



**DEERFIELD CRA PARKING LAYOUT OPTIONS
SINGLE FAMILY RESIDENCE PROPERTY**

Figure 5.7 – Single Family Lot Parking Options

5.8 Community/Parking Facility Mixed Use Project

For budgetary purposes, this potential for a one-story, mixed use facility located on the Main Beach Parking Lot was explored. A facility of this nature would include underground parking, mixed with a one-story structure that includes both parking and community facilities. This facility would provide the City with the ability to relocate the fire station and beach rescue station into this proposed building to allow for those facilities to be brought up to today’s standards. For the purposes of this preliminary study, the fire station was estimated for 3 bays and 8,000 total square feet at \$235/SF and the beach rescue for 2,000 total square feet at \$180/SF. The inclusion of 10,000 SF on the first floor for community facilities will yield a net parking gain of approximately 100 spaces.

5.9 Cove Improvements

The CRA is currently contracted with Keith and Associates for improvements to the Cove Shopping Center Parking Area. The improvements include resurfacing, striping, installation of landscaping, irrigation, lighting, drainage and the construction of The Cove entryway feature.

5.10 Budget Level Cost Estimates

The estimated construction costs for the recommended improvements to the existing parking facilities within the CRA are listed within Table 5.9 below. These recommendations should be only used for budgetary and planning purposes. Detailed engineering review will be required as each individual parcel undergoes the redevelopment process.

Item	Total Cost
Parking Lot A	\$249,600
Purchase of Property (Residential adjacent to Parking Lot A)	\$750,000
North Parking Lot	\$1,372,280
Closure of SE 1st Street Parking Lot Combination	\$1,625,000
Purchase of Typical Residential Lot	\$600,000
Residential Lot Parking Lot	\$105,000
Community/Parking Facility Mixed Use (Debt Services)	\$600,000

6 IMPROVEMENTS TO PARK FACILITIES

One of the keys to urban redevelopment is to allow for the common use of public space. Open space increases the quality of life for residents and visitors alike.

6.1 Existing Park Facilities

There are only two land uses designated as open space within the CRA. One is the site of a current parking facility, which is the largest in the Beach District, and the other is Sullivan Park. Sullivan Park currently includes shuffle board courts, a restroom building, and a parking area. Any redevelopment of the park property would require public outreach to determine the highest and best use of the property. For budgetary purposes, conceptual plans for varying levels of intensity in the park have been developed.

6.2 Option #1 Improvements

In order to maintain a similar level of intensity, Option #1 developed for Sullivan Park includes a renovation of the existing restroom and shuffle board courts as displayed below in Figure 6.2. The addition of a volleyball court, picnic amenities, and a tot lot add to the property's value as a connector to the boat shuttle. Finally, the addition of a measured (1/4 mile) running path and exercise equipment increases the use from adjacent residents.

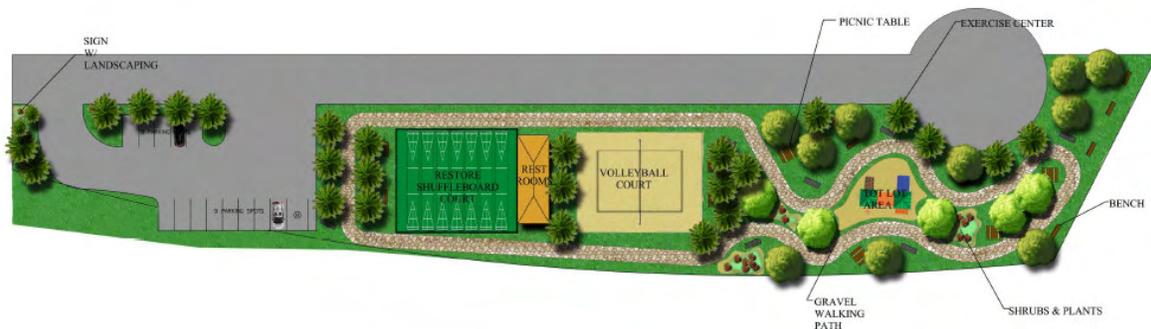


Figure 6.2 – Park Option #1

6.3 Purchase of Local Commercial Property

Sullivan Park itself is too narrow for many uses, but the acquisition of additional property in the area would allow for expansion. The expansions of the park facility may not yield additional intensity, but add to the overall aesthetics of the area and to serve as connector to the Deerfield Island boat shuttle and other adjacent uses.

6.4 Option #2 Improvements

Option #2 is contingent upon the ability of the City to obtain adjacent properties for the expansion of Sullivan Park. Option #2 Improvements are displayed below within Figure 6.3. This option would add a new municipal pool and multiple pavilions to Sullivan Park

along with the other facilities mentioned in Option 1 and serve as connector to the Deerfield Island boat shuttle and other adjacent uses.



Figure 6.4 – Park Option #2

6.5 Option 3 Improvements

Option 3 takes a departure from the concept of the park being an accessory to the boat shuttle. Option 3 would take advantage of the existing marine concepts to provide for a boat ramp, trailer parking, and municipal docks as displayed below in Figure 6.5.



Figure 6.5 – Park Option #3

6.6 Pier Enhancements

The CRA is currently under contract for a developer to improve the facilities at the Deerfield Beach Pier. As these improvements are vetted out a more detailed budget will be available.

6.7 Beach Improvements

Each year, the CRA budgets for various improvements to the beach, including upgrades to street furniture (trash cans, benches, bike racks, etc.). These improvements are budgeted each year and spent on an as needed basis.

6.8 Budget Level Cost Estimates

The estimated construction costs for the recommended improvements to the existing park facility within the CRA are listed within Table 6.6 below. These recommendations should be only used for budgetary and planning purposes. Detailed engineering review will be required as each individual parcel undergoes the redevelopment process.

Item	Total Cost
Park Option #1	\$332,118
Park Option #2	\$1,839,903
Park Option #3 Debt Service	\$300,000
Purchase of Lot adjacent to Sullivan Park for Options 2 & 3	\$2,500,000

7 LIGHTING IMPROVEMENTS

Lighting is a key for both safety and aesthetics. In areas of urban redevelopment, lighting levels are particularly important because of the high level of interaction between pedestrian and vehicular traffic. Areas of commercial redevelopment are also conscious of lighting levels between their areas and the parking facilities required to support them.

7.1 Existing Lighting

There are approximately 160 street lights within the CRA, varying for cobrahead lighting for vehicular traffic to specialized turtle compliant fixtures along the boardwalk as displayed below within Figure 7.1.



Figure 7.1 – Existing Lighting

7.2 Lighting Coverage Improvements

In reviewing the areas that have insufficient coverage within the CRA, there are approximately 57 locations that can benefit from additional lighting. One of the largest expenses with lighting is the ability to acquire easements/access to power the lights. One of the products that Chen and Associates has successfully utilized in the past is solar powered lighting. In addition to lower life cycle costs, the advantage of these lights after a power outage as a result of a hurricane or other disaster cannot be underemphasized.

7.3 Beach Area Turtle Compliant Upgrades

The Florida Fish and Wildlife Commission have become increasingly stricter in its interpretation of the Florida Department of Environmental Protection’s rules relating to the protection of sea turtles in nesting season. Chen and Associates is currently implementing almost two miles of retrofit along the beach in the City of Fort Lauderdale

and is intimately familiar with turtle compliant fixtures. The addition of spot lighting from bollards with LED lights and other technologies can also be explored to gain the desired level of lighting.

7.4 Turtle Nesting Compliant Upgrades

In order to comply with an FFWC order, the CRA has budgeted temporary improvements to the lighting in order to make the area turtle compliant.

7.5 Budget Level Cost Estimates

The estimated construction costs for the recommended improvements to the existing street lighting infrastructure within the CRA are listed within Table 7.4 below. The recommended improvements assume private developments do not upgrade lighting facilities. These recommendations should be only used for budgetary and planning purposes. Detailed engineering review will be required as each individual parcel undergoes the redevelopment process.

Table 7.5 – Estimated Budget for Lighting Improvements			
Item	Quantity	Unit Cost	Total Cost
Additional Street Lights	57	\$9,000	\$513,000
Beach Area Light Improvements	1	\$750,000	\$750,000
Turtle Compliance	1	\$400,000	\$400,000
		Subtotal:	\$913,000
		Design Cost:	\$136,950
		Construction Administration:	\$45,650
		Contingency:	\$91,300
		Total:	\$1,186,900

REQUESTED ACTION:

Award Cove Shopping Center Parking Lot Improvements Construction Services contract to low bidder West Construction in accordance with bid specifications and direct staff to commence project.

SUMMARY EXPLANATION/BACKGROUND:

The CRA solicited Request for Qualifications from construction companies to build The Cove Shopping Center Parking Lot Improvements project. The seven (7) respondent firms were evaluated by a three member evaluation committee based on 1.) ability and experience, 2.) means and methods, 3.) schedule, 4.) references, 5.) compliance and financial capacity. Firms were ranked and the top three (3) firms were invited to bid on the project.

The procurement ordinance requires that award be to the low bidder, West Construction.

CRA staff, representatives from the Engineer of Record, Keith and Associates, and City of Deerfield Beach Engineering staff invited property owners, business owners and nearby residents to attend an informational meeting on August 24th to allow stakeholders to comment on the proposed project implementation means and methods, learn about an alternative phasing plan that would further reduce the total time required for project implementation, and determine a preferred project approach.

In preparation for the project, CRA staff mailed and hand-delivered Temporary Construction Easements (TCE) to the impacted property owners. These agreements allow the CRA to reconstruct portions of the sidewalk between the public right of way and privately owned buildings. At the time this staff report was prepared CRA staff had gathered 17 fully executed documents and 3 written/verbal agreements to sign the TCE. Three property owners had not affirmatively responded to the request.

This is a budgeted item. The contractor has indicated that they are ready to proceed.

CRA staff is requesting that the CRA approved a resolution wherein the CRA contracts with West Construction in an amount not to exceed \$1,864,853.69 for the base bid and an amount not to exceed \$2,167,113.13 to construct the base contract and add alternatives as outlined in The Cove Shopping Center Parking Lot Improvements Construction Services bid packet and to further direct staff to negotiate a contract with West Construction and issue a Notice to Proceed.

ATTACHMENTS:

1. Resolution
2. Relevant excerpts from the Bid Specifications / Contractor Requirements

RESOLUTION NO. 2010/_____

A RESOLUTION OF THE COMMUNITY REDEVELOPMENT AGENCY OF THE CITY OF DEERFIELD BEACH, FLORIDA, AWARDED THE CONTRACT FOR THE COVE SHOPPING CENTER PARKING LOT IMPROVEMENTS CONSTRUCTION SERVICES TO LOW BIDDER WEST CONSTRUCTION AND TO AUTHORIZE STAFF TO NEGOTIATE A CONTRACT WITH WEST CONSTRUCTION FOR AN AMOUNT NOT TO EXCEED \$2,167,113.13 AND IN ACCORDANCE WITH PROJECT BID DOCUMENTS

WHEREAS, the CRA wishes to improve the Cove Shopping Center economically and aesthetically,

WHEREAS, the CRA Plan identifies the redevelopment of The Cove Shopping Center as a priority redevelopment project;

WHEREAS, the CRA issued a Request for Qualifications to solicit highly qualified contractors to perform the scope of services contained in the Bid Document;

WHEREAS, firms who responded to the Request for Qualifications were pre-qualified by an evaluation committee; and

WHEREAS, of the pre-qualified contractors who were invited to bid on the project, West Construction was the lowest bidder;

NOW, THEREFORE, BE IT RESOLVED BY THE COMMUNITY REVELOPMENT AGENCY OF THE CITY OF DEERFIELD BEACH, FLORIDA, AS FOLLOWS:

Section 1. The above referenced "Whereas" clauses are true and correct and made a part hereof.

Section 2. The CRA hereby selects low bidder West Construction to perform the scope of services contained in Bid #.

Section 3. The CRA hereby directs staff to negotiate a contract with West Construction to perform the scope of services contained in Bid# for an amount not to exceed \$2,167,113.13.

Section 4. The CRA hereby directs staff to issue a Notice to Proceed to West Construction upon full execution of said contract.

PASSED AND ADOPTED THIS _____ DAY OF _____, 2010.

PEGGY NOLAND, CHAIR

ATTEST:

ADA GRAHAM-JOHNSON, MMC, CITY CLERK

Deerfield/Resolutions/Cove Shopping Center Reconstruction Contract Resolution

**Due to the large size of the Cove Shopping Center Parking Lot Improvements Bid Document,
this item cannot be made available online.**

To request a copy of the Bid Document, please contact the City Manager's office at

954-480-4222

Or via email at KKlopp@Deerfield-Beach.com